



**RallyClassics AFRICA 2024**  
12th - 18th September 2024

**Sporting/Technical Regulations V2**

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## 1 - PROGRAM-SCHEDULE

PROGRAMA-HORARIO / SCHEDULE v2 (06/06/2024)			
DIA	HORA	ACTO	LUGAR
15.04.24	20h00	Apertura inscripciones - <i>Entries Open</i>	www.RallyClassics.org
23.08.24	20h00	Cierre Inscripciones - <i>Entries Closing</i>	
28.08.24	20h00	Publicación Lista de Inscritos - <i>Entry List Publication</i>	Sportity & www.RallyClassics.org
09.09.24	18h00	Publicación Tramo Calibración - <i>Calibration Zone Publication</i>	Sportity
13.09.24	10h00-14h00	Verificaciones administrativas - <i>Administrative Checks</i>	Kasbah Hôtel Xaluca Maadid, Erfoud
	10h00-17h00	Verificaciones técnicas - <i>Technical Scrutineering</i> Entrega del GPS de seguimiento - <i>GPS tracking delivery</i>	
	18h00	Publicación orden salida Etapa 1 - <i>Start list Leg 1</i>	Sportity
	18h30	Foto de Grupo de Bienvenida - <i>Welcome Group Photo</i>	Kasbah Hôtel Xaluca Maadid, Erfoud
	18h50	Entrega Roadbook 1ª etapa - <i>Roadbook 1st Leg delivery</i> Tabla de medias 1ª Etapa - <i>Average Speed Table Leg 1</i>	
	19h00	Briefing Etapa 1 - <i>Leg 1</i>	
14.09.24	07h15	Entrega Carnet de ruta 1ª Etapa - <i>Time card 1st Leg delivery</i>	Kasbah Hôtel Xaluca Maadid, Erfoud
	07h30	Salida Etapa 1 - <i>Start Leg 1</i>	
	14h25	Final Etapa 1 - <i>End Leg 1</i>	
	17h30	Clasificación Etapa 1 - <i>Leg 1 Results</i> Publicación orden salida Etapa 2 - <i>Start list Leg 2</i>	Sportity
	18h50	Entrega Roadbook 2ª etapa - <i>Roadbook 2nd Leg delivery</i> Tabla de medias 2ª Etapa - <i>Average Speed Table Leg 2</i>	Kasbah Hôtel Xaluca Maadid, Erfoud
	19h00	Briefing Etapa 2 - <i>Leg 2</i>	
15.09.24	07h15	Entrega Carnet de ruta 2ª Etapa - <i>Time card 2nd Leg delivery</i>	Kasbah Hôtel Xaluca Maadid, Erfoud
	07h30	Salida Etapa 2 - <i>Start Leg 2</i>	
	14h00	Final Etapa 2 - <i>End Leg 2</i>	
	17h30	Clasificación Etapa 2 - <i>Leg 2 Results</i> Publicación orden salida Etapa 3 - <i>Start list Leg 3</i>	Sportity
	18h50	Entrega Roadbook 3ª etapa - <i>Roadbook 3rd Leg delivery</i> Tabla de medias 3ª Etapa - <i>Average Speed Table Leg 3</i>	Kasbah Hôtel Xaluca Maadid, Erfoud
	19h00	Briefing Etapa 3 - <i>Leg 3</i>	
16.09.24	07h15	Entrega Carnet de ruta 3ª Etapa - <i>Time card 3rd Leg delivery</i>	Kasbah Hôtel Xaluca Maadid, Erfoud
	07h30	Salida Etapa 3 - <i>Start Leg 3</i>	
	14h30	Final Etapa 3 - <i>End Leg 3</i>	
	17h30	Clasificación Etapa 3 - <i>Leg 3 Results</i> Publicación orden salida Etapa 4 - <i>Start list Leg 4</i>	Sportity
	18h50	Entrega Roadbook 4ª etapa - <i>Roadbook 4th Leg delivery</i> Tabla de medias 4ª Etapa - <i>Average Speed Table Leg 4</i>	Kasbah Hôtel Xaluca Maadid, Erfoud
	19h00	Briefing Etapa 4 - <i>Leg 4</i>	
17.09.24	07h15	Entrega Carnet de ruta 4ª Etapa - <i>Time card 4th Leg delivery</i>	Kasbah Hôtel Xaluca Maadid, Erfoud
	07h30	Salida Etapa 4 Marathon - <i>Start Leg 4 Marathon</i>	
	14h00 - 17h00	Remote Service	Hôtel Tombouctou, Merzouga
	18h30	Final Etapa 4 - <i>End Leg 4</i>	Kemkemia Luxe Noir, Erg Chebbi
	19h30	Clasificación Etapa 4 - <i>Leg 4 Results</i> Publicación orden salida Etapa 5 - <i>Start list Leg 5</i>	Sportity
	19h50	Entrega Roadbook 5ª etapa - <i>Roadbook 5th Leg delivery</i> Tabla de medias 5ª Etapa - <i>Average Speed Table Leg 5</i>	Kemkemia Luxe Noir, Erg Chebbi
20h00	Briefing Etapa 5 - <i>Leg 5</i>		
18.09.24	07h30	Foto de Familia en las Dunas - <i>Family Photo in the Dunes</i>	Kemkemia Luxe Noir, Erg Chebbi
	07h45	Entrega Carnet de ruta 5ª Etapa - <i>Time card 5th Leg delivery</i>	
	08h00	Salida Etapa 5 - <i>Start Leg 5</i>	
	13h00	Final Etapa 5 - <i>End Leg 5</i>	Kasbah Hôtel Xaluca Maadid, Erfoud
	19h00	Publicacion Resultados Finales - <i>Final Results</i> Entrega Trofeos - <i>Prizes Giving Ceremony</i>	
	20h30	Cena-Dinner End of RallyClassics Africa	

WWW.RALLYCLASSICSAFRICA.COM



## 2 - ORGANIZATION

### 2.1 - DEFINITION

The **RallyClassics Club** organizes the **RallyClassics AFRICA 2024**, a Regularity and Navigation Raid for vehicles with 4 or more wheels.

The event shall be carried out with this Regulation, any Bulletins, and annexes.

#### 2.1.1 - ORGANIZING COMMITTEE

Alex Romani	President of the RallyClassics Club
Fina Roman	Local coordination
Christian Gutierrez	Logistics Manager
Josep M <sup>a</sup> Servià	Itinerary Manager
Tere Armadans	Liaison Officer
Juan Manuel Segura	Sporting Area
Ramon Garcia	Timekeeping Manager
Sergi Blasco	Press Officer

#### 2.1.2 - PRINCIPAL OFFICIALS

Race Director:	Juan Manuel Segura
Stewards:	Alex Romani
	Ramon García
	Fina Roman
Secretary:	Tere Armadans
Medical chief:	Dr. Santi Sabatés
Track Manager:	Jordi Estaper

### 2.2 - EVENT FORMAT

The **RallyClassics AFRICA 2024** will take place between **12 and 18 September 2024, distributed** in 5 Legs.

Participants will be faced with:

- RZ = Regularity Zone: where they must try to approximate the ideal passage time by following the average speeds imposed.
- NZ = Navigation Zone: where they must respect the itinerary indicated in the RoadBook and validate the waypoints (WP).

In each Leg there may be several RZ and NZ stages. The itinerary to be followed will be detailed in the RoadBook and Time Card.

## 3 - ALLOWED VEHICLES

3.1 - All vehicles must comply at all times with the regulations in force for their circulation on public roads (Circulation Permit, Compulsory Vehicle Insurance, Technical Inspection). Participants must sign an affidavit of veracity of the data filled in on the registration form, during the administrative checks. Participants will need to prove their driving licence and the validity of their insurance in Morocco.

3.2 - The following vehicles will be admitted to participate:

- All Terrain 4x4
- Modified 4x2
- Trucks
- Prototypes
- SSV

### 3.3 - Admitted vehicles will be divided into the following CLASSES:

- All-Terrain 4x4
  - Pre-1997 class (manufactured until 31/12/1996)
  - Pre-2005 class (manufactured between 01/01/1997 and 31/12/2004)
  - Post-2005 class (manufactured after 01/01/2005)
- Modified 4x2
  - Modified 4x2 class (manufactured until 31/12/2004)
- Trucks
  - Truck Class
- Prototypes
  - Prototype Class (non-series derivative vehicles)
- SSV
  - SSV Class

### 3.4 - BY CATEGORIES:

- CLASSICS
  - It includes vehicles from: All-terrain 4x4 pre-1997 class, All-terrain 4x4 Pre-2005 class, Modified 4x2 Pre-2005 and Trucks Pre-2005.
- OPEN
  - It includes the vehicles from: post-2005 4x4 All-Terrain class, Prototypes, SSVs and Post-2005 Trucks.

## 4 - ADMITTED PARTICIPANTS

### 4.1 - CREW COMPOSITION

Crews must consist of 2 members (driver and codriver) or up to 3 members for Trucks.

- The driver must have a valid driving licence for the category of vehicle he or she is driving, valid for the territory where the race is held.
- Codrivers must be 18 years of age or older.

Only registered members (driver and codriver(s)) will be allowed to occupy the vehicle. Participants are obliged, at all times, to respect the Driving Code and these regulations, as well as the instructions of the members of the organization. Failure to comply with this or any other rule may result in the exclusion of the team.

## 5 - REGISTRATION APPLICATIONS

### 5.1 – REGISTRATIONS PERIOD

The registration application period will be open from **April 15th to August 23rd, 2024, or until full capacity is reached.**

### 5.2 - LIMITATION OF REGISTRATIONS, ADMISSIONS AND CANCELLATIONS

The maximum number of participants is limited to **50 teams**, per selection, among the correctly received and paid registration requests. The organizing committee may reject any registration, with the sole duty of reimbursing the registration fees to the team. The organising committee may designate some of the registration requests as reserves.

When the list of teams admitted to participate is announced, it will be final. Teams that are not admitted will have their registration fees refunded.

If the event must be postponed or suspended, due to force majeure, participants may opt for the following formulas:

- a) Keep the full registration until the date of the actual celebration of the event.
- b) Recover 100% of the registration fee if the suspension occurs more than 2 months in advance; 75% if the suspension is between 2 months and 2 weeks before the event; or 50% if the suspension is less than two weeks before the event.

If a team has to cancel its participation due to justified force majeure, it can opt for the following options:

- c) Save the amount paid on account of the registration of the next edition
- d) Recover 100% of the amount paid if the cancellation occurs up to 2 months before the start of the event; 75% of the amount paid if the cancellation occurs between 2 months and 1 month before the start of the event; 50% if it occurs before the closing of registrations.
- e) Once registration is closed, there will be no refund to the team that causes withdrawal.

### 5.3 - PROCEDURE

- 1st. Fill in all the details of the registration application form available on the website <https://www.rallyclassics.club/rallyclassics-africa-2024/>
- 2nd. Make payment by **card** through a payment gateway when registering the registration application or by **bank transfer** in favour of Club RallyClassics to:  
BBVA - **ES48 0182 9764 2302 0000 1717**  
SWIFT CODE: BBVAESMMXXX  
indicating the name of the first driver in the concept
- 3rd. **Send the receipt by e-mail to africa@rallyclassics.org**  
All this must be received by RallyClassics within a maximum period of 15 days after the registration request has been made.

### 5.4 - REGISTRATION FEES

#### 5.4.1 - The entry fee is set out in:

- Registration fee **€2,990** (discounted registration until 31/05/2024, **€2,490 \***)  
\*Only applicable to registrations paid in full before the end of the bonus period
- **Accommodation 1200€** (2pax)
  - in a double room on a half-board basis – drinks not included.
    - 6 nights at Hotel Xaluca Erfoud (from 12 to 18 September, excl.17/09)
    - 1 night at Haima Kemkemia Luxe Noir (September 17)
- **Insurance 100€/person**, see article 6.2
- **ANUBE Sport Tracking** (STELLA + SPICA): **350€ per vehicle** (installation kit not included)

#### Registration includes:

- Participation rights
- 5 RoadBooks, one per Leg
- GPS Safety Tracking
- 2 race number stickers
- 1 Rally plate sticker

- 2 small sticker plates
- Identification bracelet (1 per person)
- Computerized classifications
- Race Documentation & Local Permits
- Trophies for the Final and Leg Winners
- Finishers for teams that complete the event
- Merchandising RallyClassics África
- Secretary, website and Sportity bulletin board

**OPTIONS:**

- Digital Photo book: 200€
- Upgrade to single rooms, according to demand and availability.
- Digital roadbook on Anube's Tablet: 250€ + deposit of 300€ (includes installation kit)
- Local support team: €500 (per team)

**5.4.2 - Assistance Registrations:**

For assistance/workshop vehicles to access the Service Park, they must make a separate registration.

- Registration fee: **900€** (for each assistance vehicle).
- Accommodation **1.100€** (2pax)  
7 nights (from 12 to 18 September) Hotel Xaluca Erfoud  
Double room on a half-board basis – drinks not included
- Insurance **100€/person**, see article 6.2

Registration includes:

- Access to the service park
- 1 Rally plate sticker
- 2 small sticker plates
- Identification bracelet (1 per person)

Options:

- Upgrade to single rooms, according to demand and availability.
- Haima double occupancy Kemkemia Luxe Noir - 1 night (17/09) with dinner and breakfast:  
125€/person
- Merchandising RallyClassics Africa: 25€/each

Vehicles registered as assistance may provide mechanical tasks to more than one team.

**5.4.3 – Companions:**

Companions who wish to have access to the optional services offered during the event must make a separate registration.

- Registration fee: free of charge
- Insurance **100€/person**, see article 6.2

Options:

- Accommodation **1.100€** (2pax)  
7 nights (from 12 to 18 September) Hotel Xaluca Erfoud.  
Double room with breakfast and dinner – drinks not included
- Upgrade to single rooms, according to demand and availability

- Haima double occupancy Kemkemia Luxe Noir - 1 night (17/09) with breakfast and dinner: 125€/person
- Merchandising RallyClassics Africa: 25€/each

## 6 - INSURANCE

6.1 - In addition to the Compulsory Insurance required for each vehicle, the Organization has a Civil Liability insurance with a limit of €600,000.

6.2 RallyClassics has taken out a basic accident insurance (excluding competition) and repatriation, which **all members** of each team (crew, mechanics, companions, press) must subscribe by paying (**100€/person**) during the registration process.

6.3 - All teams are free to take out other insurance complementary to the one described in article 6.2 that they deem appropriate.

## 7 - CREW COMMITMENT

### 7.1 - RESPONSIBILITIES

7.1.1 - Participants will be personally liable for their own civil liability or liability to third parties and declare to release the organization from any liability arising from the use of their vehicle, whether for material or bodily damage of which they could be victims or perpetrators, by the mere fact of registering for the event.

7.1.2 - Teams are required to communicate their abandonment or their intention to continue in the rally after not taking part in any of the regularity sections, by means of Stella GPS messaging, by telephone to the SOS number or in person at the rally HQ.

7.1.3 - The participating teams must be aware that any problems that arise during the celebration of the rally must be solved by their own team (breakdowns, losses in the route, accidents, punctures, lack of fuel, etc.), preparing the vehicle thoroughly, and/or with the registration of an assistance team.

7.1.4 - The management of alternative routes or abandonment must be carried out by the participating team itself. The function of the broom car provided by the organization is to know the situation of the teams that leave (especially in areas without telephone coverage) and, if feasible, to solve eventualities. If no member of the team is found next to a vehicle found within a regularity section or liaison sector, the team will be considered to have abandoned and not need any assistance.

### 7.2 - EMERGENCY KIT

Each vehicle must have an EMERGENCY KIT, which can be checked at any time, consisting of:

- Mobile phone with coverage in Morocco
- Medicine kit
- 3 liters of drinking water
- Survival food rations (e.g. energy bars, nuts, etc.),
- Work gloves
- 1 spare wheel MANDATORY, recommended 2 wheels
- Minimum 8m sling and 2 shackles
- Shovel
- 2 flashlights
- 2 grit strips
- Belt-trimmer accessible to driver and co-driver
- Anti-blizzard or work goggles
- Mechanical or hydraulic jack

7.3 - In Parc Fermé and service areas, as they are areas open to the public, the organization will not be responsible for the objects that may remain in the vehicles, nor for the vehicles themselves; each team will be responsible for locking their vehicle properly and removing any valuables from inside.

## 8 - OFFICIAL BULLETIN BOARD

The Official Bulletin Board will be through the **Sportity app**.

To receive information directly on your mobile phone, you must download the Sportity app and enter your password: **RCAFRICA**



## 9 - GPS and DIGITAL ROADBOOK on TABLET

9.1 - Each team will carry two GPS tracking devices from Anube Sports (Stella and Spica) necessary for the timing and for the safety monitoring of all participants.

9.2 – Anube Sports will make available a limited number of 35 Tablets (for digital Roadbook), which must be ordered when registering. The installation kit for the tablet will be provided by Anube Sports, with a deposit of €300, which will be returned when the kit is returned in the same condition in which it was delivered.

9.3 - Each participating team is responsible for the proper use of the GPS and Tablets (in case of requesting the digital RoadBook on Tablet), and for keeping them placed in the indicated place in a safe way.

9.4 - At the end of the race or in case of abandonment, each participating team must return the devices to the Secretary/ANUBE. By doing so and if the devices are in good condition, the deposit deposited will be returned. Participants should familiarize themselves with the use of GPS and Tablets, through the information links (see Annex 2), through consultations with the supplier, or ultimately in the training course that will take place on September 13 at the Hotel Xaluca Erfoud after the verifications.

The digital Roadbook of each Leg will be activated by a password that will be published on the notice board (Sportity) daily at the same time as the delivery of the paper RoadBooks.

## 10 - EVENT DEVELOPMENT

### 10.1 - DESCRIPTION

The teams must follow the route marked on the Road-Book at all times.

### 10.2 - BRIEFING

Before each leg, a briefing will be held, which will be mandatory for all teams. The place and time will be as detailed in the Program-Schedule.



### 10.3 - AVERAGE SPEED AND REGULARITY

The teams must circulate respecting the traffic rules at all times and comply with the average imposed by the organization.

In the regularity sections (RZ) the average imposed may be **up to 75 km/h**.

There will be a single average for all participants.

### 10.4 - DZ-FZ LIMITED SPEED ZONES

The passages through the towns will be especially observed, where the teams must take extreme caution. For this purpose, there may be SPEED LIMITED ZONES, indicated as DZ>FZ (and the maximum authorized speed), failure to respect them could be grounds for exclusion.

These zones may be within an RZ or on a link.

If one of these zones is within an RZ, there will be no Secret Controls in the previous 300 meters and 500 meters after.

### 10.5 - OFFICIAL TIME

The official time of the race will be the one provided at the Start Time Control of each Leg.

### 10.6 - CALIBRATION SECTION

It will be published on the website and on the notice board.

### 10.7 - INTERVAL BETWEEN TEAMS and STARTING ORDER

At the start of each leg, the interval between each team will be 1 minute.

At least 1 minute must be maintained between departures of each RZ and NZ.

The starting order of the 1st Leg will be made in order of race number, the rest of Legs may be in order of Provisional Classification, Classification Order of the previous Leg or chosen by order of Classification.

The format chosen for each Leg will be communicated by means of a complement.

### 10.8 - LEG START

At the start of each leg, all teams will receive a Road Card, with the time to be invested between the Time Controls.

10.8.1 - Participants must start the **first RZ or NZ** of each Leg at their assigned time on the Road Card, and may not start at any other time.

10.8.2 - Any team that cannot take the leg start at its established time will have a maximum of 15 minutes after the start time of the last participant who starts the leg to do so.

10.8.3 - Each leg will start with a Time Control (CH). The penalties described in Article 10.9.1 shall apply

### 10.9 - TIME CONTROLS (CH)

A physical Time Control at the start of each leg -and other virtual time controls-, will serve to verify that the teams have gone through the proposed itinerary, and to determine the penalties that must be applied to each team. This route must be covered in its entirety in the times determined by the Route Card and with an average speed lower than or up to 60 km/h.

#### 10.9.1 - PENALTIES IN TIME CONTROLS

At the physical Time Controls (CH) there will be a commissioner, who will attest to the time of passage of each team, and will record it in his notebook. Such registration shall be final. The team that arrives late to the Time Control, the steward will assign a new start time, from which the team must make its calculations to face the rest of the route.

Each team must pass the Control banner within its minute (e.g.: if its theoretical time is 08:34, it will have the 60 seconds between 08:34:00" and 08:34:59").

10.9.2 - Entering a Time Control (CH) early will result in a penalty of 10 points per minute or fraction.

10.9.3 - Entering a Time Control (CH) late will result in a penalty of 5 points per minute or fraction.

10.9.4 - At the end of each leg, only delay is penalized, at the rate of 5 points per minute or fraction (overtaking is allowed).

10.9.5 A delay **of more than 15 minutes** in any Time Control will incur a maximum penalty of **75 points**.

10.9.6 - AUTOMATIC OVERRIDE OF A TIME CONTROL

If 20% or more of the equipment is penalized for delay in the same Time Check, this control will be automatically cancelled.

10.10 - REGULARITY ZONES (RZ)

10.10.1 - In all RZ, the average speeds imposed must be respected.

10.10.2 - **The maximum speed** at which an RZ may be driven shall be **less than 150%** of the average speed imposed. Exceeding the maximum speed for more than 5 seconds at a time will result in a penalty.

10.10.3 - FREE SELF-EXIT

At the start of the RZ there will be no physical Time Control, and the teams will NOT have a pre-established departure time; They will take the start when they are ready to do so, and as long as there is no equipment at the starting point. The section will close 3 hours after the theoretical time of passage of the first participant. The previous waiting order must be respected at the exit of each RZ with self-departure.

The Auto-departure can be done at any minute, but always in the 00 second". As an example, passing the RZ starting point at the 55" or 59" second will be considered as the team having started in the previous minute. Once the starting point (SRZ) has been passed, it will be considered as a section started, and it will not be allowed to go back to start it again

10.10.4 - It is strictly forbidden to stop inside the RZ, except for breakdown or force majeure.

10.10.5 - Unless expressly authorised by the Race Direction, the use of ouvriers or assistance (support teams, friends, spectators and local mechanics, etc.) circulating on any RZ is strictly prohibited, under penalty of exclusion.

10.10.6 - Any team that does not go through an RZ (or part of it) will be able to follow in the event. Penalties will be applied for each missed check.

10.10.7 - It is strictly forbidden to drive in the opposite direction to that provided in the RoadBook.

10.11 - SECRET CONTROLS REGULARITY (CS)

10.11.1 - The secret controls will open and close virtually, 15 minutes before and after the theoretical time of passage of each participant, unless indicated by the direction of the rally. Passing with the Control closed will apply the same penalty as if it had not passed.

10.11.2 - There will be approximately one Secret Control for every km of RZ. The secret controls located on the RZ will take the times to the second, with penalties being one (1) point for each second difference from the scheduled passing time.

10.11.3 - The maximum penalty that will be applied when a team passes through a CS, either by overtaking or by delay (before being virtually dismantled) will be **30 points**.

10.11.4 - Failure to go through a CS (or to do so before opening or after virtually closing) will be penalized with **60 points**.

10.11.5 - Secret controls (CS) will never be located within 500 m after the start, or a STOP, or warning signs (!, !!, !!) marked on the Road-Book, to allow the teams to recover the average without putting mechanics or traffic at risk. There will also be no CS in areas with imposed averages below 20 km/h.

10.12 - NAVIGATION ZONES (NZ) In the Navigation Zones (by dunes or oueds, etc.) the teams must orient themselves by calculated directions, and find the different WayPoints proposed by the organization, shown in each panel of the RoadBook.

10.12.1 - All Waypoints must be validated, **failure to pass through each of the WayPoints (WP)** will be penalized with 60 points.

10.12.2 - A maximum time will be set for completing each NZ, after which it will be penalized at the rate of 5 points per minute of delay. A delay **of 30 minutes** at the end of an NZ will result in a maximum penalty of **150 points**.

10.12.3 - The fact of finishing a NZ more than 30min late will mean that the team will not be able to continue with the leg normally. The Race Direction must be contacted to coordinate at which point of the route they could rejoin the caravan, if possible.

#### 10.13 - PASSAGE CONTROLS (CP)

If Checkpoints (CP) are established, these will be indicated in the RoadBook and on the Route License. Failure to have a visa at these checkpoints will result in a penalty of 100 points for each.

#### 10.14 – NEUTRALISATION ZONE (NEU)

The neutralization zones will be defined as NEU IN and NEU OUT.  
The minimum neutralization time must be respected.

#### 10.15 – GAS STATIONS

10.15.1 - The amount of fuel in the vehicle at the start of each Leg shall offer a **minimum range of 250 km**. For vehicles with less autonomy, jerrycans must be provided, and they must be well fastened on the outside of the vehicle.

10.15.2 - It is recommended to refuel at the end of each Leg, at the junction between the last section and the CH at the end of the leg.

#### 10.16.- SERVICE AREA

The authorized assistance area will be the one set up in the parking lot of the Kasbah Hotel Xaluca Maadid Erfoud.

10.16.1 - It may only be carried out by the crew members of each team or by the means of mechanics duly registered in accordance with Article 5.4.2.

10.16.2 - At the end of each Leg and once inside the service area, it will not be possible to leave it unless prior authorization is given by the Rally Management.

10.16.3 - Refuelling is not permitted in the service area.

10.16.4 - Cleaning of vehicles must be carried out during the liaison, or once inside the Assistance Zone.

10.16.5 - For the Marathon Leg, a special area will be set up as a Remote Service where teams can receive assistance. The rules described in Articles 10.16.1, 10.16.3 and 10.16.4 apply.

It is allowed to leave the Remote Service to refuel, clean and refresh. In this case, you must return at least 15 minutes before the departure time of the Time Control.

## 10.17 - TIMEKEEPING

### 10.17.1 - MEASUREMENTS AND TIMING

The Rally was measured with Garmin Glo GPS.

The timing will be carried out by ANUBE Sports' GPS system, to the second.

### 10.17.2 - INTERRUPTION OF AN RZ

If for any reason a RZ cannot be disputed by several teams, the best times may be given and the rest of the teams may receive the penalty of the 10th classified.

### 10.17.3 - TIME ALLOCATED BY THE COLLEGE OF SPORTS COMMISSIONERS

If for any reason a time of a participant cannot be taken, the College of Sports Commissioners will make the most equitable and fair decision to award said time, which will be final.

### 10.17.4 - "JOKER"

In RallyClassics Africa 2024 there will be NO "Joker" app in Secret Controls.

## 11 - CLASSIFICATIONS

11.1 - The team that accumulates the fewest points will win, after adding the penalties that would apply in the Regularity Zones (RZ), the Navigation Zones (NZ), the Time Controls (CH) and the Checkpoints (CP).

After the publication of the informal classifications of each Leg there will be a period of 30 minutes for possible complaints (in writing and with a deposit of €300) after which the classification will be official and unappealable.

### 11.2 - LEG WINNING TROPHY

Recognition will be given to the team that has accumulated the fewest penalty points for each of the Legs. It will be delivered during the daily briefing.

### 11.3 - GENERAL CLASSIFICATION

**Only vehicles in the CLASSICS Category will be eligible for the General Classification.**

11.4 - To be eligible for the trophies, the entire team (driver and co-driver) must be present at the trophy ceremony. Failure to attend this event will result in the forfeiture of these trophies.

- For the first three teams in the General Classification (CLASSICS Category)
- For the top three finishers in each of the Classes:
  - CLASSICS 4x4 pre-1997
  - CLASSICS 4x4 pre-2005
  - CLASSICS MODIFIED 4X2
  - TRUCK
  - OPEN

- The #MotorDona trophy will be awarded to the first (1st) female driver in the General Classification, among all the participants.

## 12 - PERMANENT OFFICE

Until Tuesday, September 10, 2024, it will be located at:

Plaça Països Catalans, 49 – 08410 Vilanova del Vallès – Barcelona/Spain(tel. 938 458 630)

From September 13, 2024, the Rally Base (HQ) will be located at:

Kasbah Hotel Xaluca Erfoud - Km 5 Route d'Arfoud à Errachidia, B.P. 205, Erfoud 52200 Morocco -

31°29'13.3"N - 4°12'47.6"W



## 13 - FINAL PROVISION

Any other matter not contemplated in these Specific Regulations will be decided by the College of Sports Stewards.

## 14 - TECHNICAL REGULATIONS

### 14.1.- LIGHTING SYSTEM

For the CLASSICS Category, the use of LED bars is not allowed, LED lights are allowed inside the original or auxiliary optics, as long as they do not break the classic aesthetics of the vehicle.



For the **OPEN** Category, LED bars are accepted.

### 14.2 - SCRUTINERINGS

Before the start of the event there will be a mandatory technical check. The scrutineering process of the vehicles shall consist of verifying, among other things:

- Tires
- Triangles (2)
- Reflective Vest (2)
- The correct placement of the race numbers and mandatory advertising of the race
- Correct installation of lower guards (crankcase, differential, etc.)
- A fire extinguisher weighing at least 2 kg.
- Skirts to prevent rocks from being thrown from the drive wheels
- Tow: place & Signalling Hooks
- Emergency Kit detailed in Article 6.2

### 14.3 – SAFETY EQUIPMENT

#### MANDATORY:

- In vehicles equipped **with** roll bars, crews **must wear an Homologated helmet.**

#### HIGHLY RECOMMENDED:

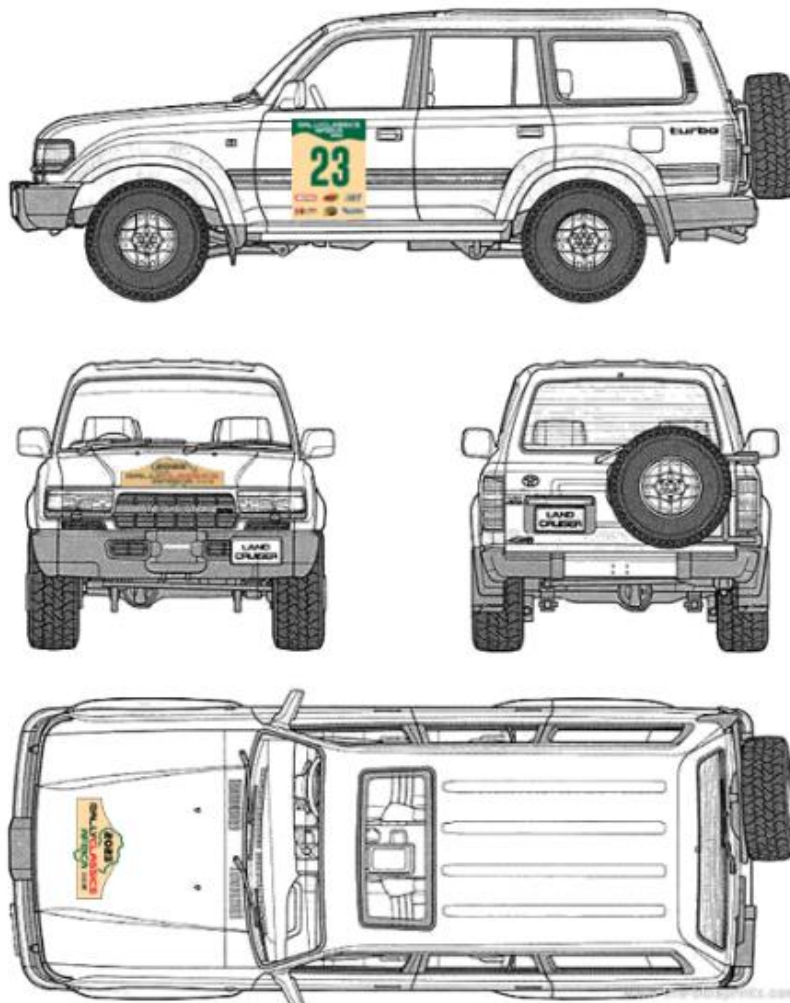
- Helmet on vehicles without bars
- Fireproof overalls
- Gloves

#### 14.4 – RACE NUMBERS & PLATES

Each team shall be provided with advertising and identification stickers, which shall be affixed to their vehicles in the positions indicated prior to the technical checks.

Cutting or handling the adhesives is not permitted.

- Rally plate 40cm x19cm
- Side car number 40cm x55cm



### ANNEX 1 – PENALTIES SUMMARY

9.3	Manipulation of the GPS or Tablet, or lack of Fair-Play at the event (especially blue flag, lack of help, etc.)	At the discretion of RC, until disqualification, and non-acceptance of registration in the following RallyClassics events
10.2	Failure to attend the Daily Briefing	Not authorized to start the next leg + 300€
10.4	Failure to respect the speed limit imposed in the DZ-FZ zone	From 3 to 20 km/h: 1st time, 100p + 100€; 2nd time 300p + 300€; 3rd time disqualification. More than 20 km/h: 1st time 300p + 300€; 2nd time 500p + 500€; 3rd time disqualification.
10.9.2	Advance in a Time Control	10 points for each minute or fraction
10.9.3	Delay in a Time Control	5 points for every minute
10.9.4	Delay in the End of Leg Time Control	5 points for every minute
10.9.5	Exceed maximum delay in Time Control (15 min).	75 points
10.10.2	Exceeding the maximum speed in RZ for 5 seconds or more	1st time 50 points; from 100 points to disqualification, at the discretion of RC
10.10.4	Stopping without justification in regularity section (RZ)	At RC discretion, up to disqualification
10.10.5	Presence of Ouvreur or Assistance in regularity sections	At RC discretion, up to disqualification
10.11.1	Going through a CS while being virtually closed	60 points
10.11.2	For every second of difference to the ideal time in the regularity section	1 point
10.11.3	Maximum delay in Secret Regularity Control	30 points
10.11.4	Secret Regularity Control omitted (for each)	60 points
10.11.7	Driving in the opposite direction to that stated in the RoadBook.	At RC discretion, up to disqualification
10.12.1	Lack of validation of a WP in NZ	60 points each
10.12.2	Exceeding the maximum time to complete an NZ	5 points (for each minute)
10.12.2	30-minute delay from maximum time in NZ	150 points
10.13	Skip Step Control (for each)	100 points
10.16	Receive Assistance Outside the Permitted Zone	At RC criteria, minimum of 500 points (with authorization from RC), up to disqualification



## ANNEX 2 – ANUBE DEVICES

ANUBE has an installation kit to take the current directly from the 12v battery to the dashboard, where both GPS will be located within reach of the pilot and co-driver. The installation kit must be plugged into 12V directly from the 12v battery taken from the dashboard. The installation of these systems must be carried out prior to the technical verifications.

Participants must purchase the installation kits of Stella and Spica from Anube.

- [Kit de Stella](#)
- [Spica Support](#)
- [Cable de alimentación Spica](#)

Anube contact: [info@anubesport.com](mailto:info@anubesport.com)

Tel: + 34 986 124 542

\*Additional Stella Device Information:

1. [Brefing Stella 3 Evo](#)
2. [Stella EVO Manual Installation 4 wheels](#)
3. [Stella EVO Manual Installation 2 wheels](#)
4. [Video Manual Stella EVO English](#)

\*Electronic Tablet Roadbook Information:

- [Electronic Roadbook Manual](#)

The devices will not be installed in installations with a voltage other than 12V, so in those vehicles with batteries of different voltage, the participant must have previously installed the appropriate converter.

The socket for the Stella should not be made after a power cut, but direct to the battery, with a 5 Amp fuse (in the installation manual of the Stella it speaks of 3A, but in this case Stella+Spica is installed).

### ANNEX 3 – WAYPOINT TYPES

	WAYPOINT	Rayon d'ouverture Opening Radius Radio de apertura	Rayon de validation en mètres Validation Radius in metres Radio de validacion en metros
Navigation WP	WPE	Prev (1000 m)	90
	WPN	800 m	200
	WPM	800 m	90
	WPS	1000 m	30
	WPC	300 m	100
	CH	1000 m	90
Start/Finish/Zone WP WP Zone/Départ/Arrivée	WPV	Open	200
	SRZ	Prev (1000 m)	30
	FRZ	800 m	90
	DZ	1000 m (Prev)	90
	FZ	Prev (1000 m)	90
	NEU IN	1000 m (Prev)	90
	NEU OUT	Prev (1000 m)	90
<b>Prev (1000 m)</b>	= waypoint précédent ou dans un rayon de 1000 m si le waypoint précédent a été manqué. = previous waypoint or within a radius of 1000 m if the previous waypoint was missed. = waypoint anterior o en un radio de 1000 m si se ha omitido el waypoint anterior.		
<b>1000 m (Prev)</b>	= dans un rayon de 1000 m ou waypoint précédent seion la distance la plus courte. = within a radius of 1000 m or previous waypoint, whichever distance is shorter. = en un radio de 1000 m o el waypoint anterior, la distancia que sea menor.		

- **WPE (WAYPOINT ECLIPSE / ECLIPSE WAYPOINT / WAYPOINT ECLIPSADO)**

[FR] Un waypoint qui devient complètement visible sur le NAV-GPS un fois que le waypoint précédent dans l'ordre chronologique a été validé, et ce, quelle que soit la distance entre le précédent waypoint et le WPE.

[EN] A waypoint which becomes completely visible on the NAV-GPS once the previous one in the chronological order has been validated, whatever the distance between the previous one and the WPE may be.

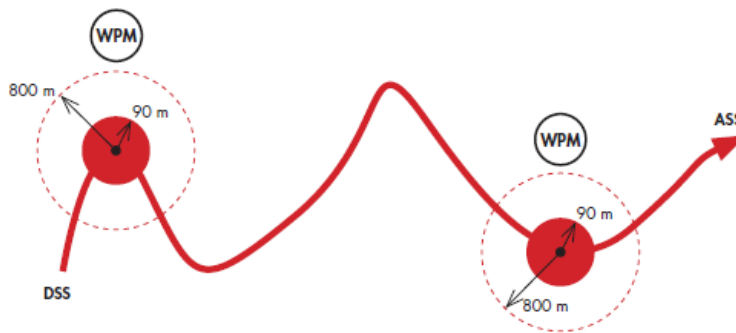
[ES] Un waypoint que se hace completamente visible en el NAV-GPS una vez validado el anterior en el orden cronológico, sea cual sea la distancia entre el anterior y el WPE.

- **WPM (WAYPOINT MASQUE / MASKED WAYPOINT / WAYPOINT ESCONDIDO)**

[FR] Il s'agit d'un waypoint standard permettant de vérifier que les instructions de navigation sont correctement suivies par les équipages. Le NAV-GPS ne dirige le concurrent vers ce point qu'une fois parvenu dans le rayon d'ouverture de ce dernier.

[EN] This is a standard waypoint to be used to check that the navigation instructions are being correctly followed by the crews. The NAV-GPS directs the competitor towards this point only once he has come within the opening radius of it.

[ES] Se trata de un waypoint estándar que se utilizará para comprobar que los participantes sigan correctamente las instrucciones del roadbook. El NAV-GPS dirige al competidor hacia este punto sólo cuando ha entrado en el radio de apertura de este.



- **WPC (WAYPOINT CONTROLE / CONTROL WAYPOINT / WAYPOINT DE CONTROL)**

[FR] Un WPC est un waypoint permettant de vérifier le respect du suivi du Road book

[EN] A WPC is a waypoint to check that the Road book has been respected

[ES] Un WPC es un waypoint utilizado para comprobar el cumplimiento del Roadbook.

- **WPS (WAYPOINT SECURITE / SAFETY WAYPOINT / WAYPOINT DE SEGURIDAD)**

[FR] Waypoint utilisé pour assurer la sécurité des concurrents et pour éviter les dangers qui peuvent être très discrets.

[EN] A waypoint used to ensure the safety of competitors and to avoid hazards that may be very inconspicuous.

[ES] Un waypoint utilizado para garantizar la seguridad de los participantes y evitar peligros que pueden ser muy poco visibles.

- **WPV (WAYPOINT VISIBLE / VISIBLE WAYPOINT / WAYPOINT VISIBLE)**

[FR] En se déplaçant vers un waypoint visible, toutes les informations sont affichées sur le NAV-GPS à tout moment lorsque le WPV est sélectionné ou que le waypoint précédent a été validé.

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[EN] Travelling towards a visible waypoint, all information is displayed on the NAV-GPS at any time when the WPV is selected or the previous waypoint has been validated.

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[ES] Al desplazarse hacia un waypoint visible, toda la información se muestra en el NAV-GPS en cualquier momento en que se seleccione el WPV o se haya validado el waypoint anterior.

- **WPN (WAYPOINT NAVIGATION / NAVIGATION WAYPOINT / WAYPOINT DE NAVEGACION)**

[FR] Il s'agit d'un waypoint standard uniquement dans les dunes permettant de vérifier que les instructions de navigation sont correctement suivies par les équipages. Le NAV-GPS ne dirige le concurrent vers ce point qu'une fois parvenu dans le rayon d'ouverture de ce dernier.

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[EN] This is a standard waypoint to be used only in the dunes to check that the navigation instructions are being correctly followed by the crews. The NAV-GPS directs the competitor towards this point only once he has come within the opening radius of it.

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[ES] Se trata de un waypoint estándar que se utilizará únicamente en las dunas para comprobar que los participantes sigan correctamente las instrucciones del roadbook. El NAV-GPS dirige al competidor hacia este punto sólo cuando ha entrado en el radio de apertura de este.

- **DZ-FZ WAYPOINTS POUR LES ZONES DE CONTROLE DE VITESSE/ ZONES DE SECURITE**
- **DZ-FZ WAYPOINTS FOR SPEED CONTROL ZONES / SAFETY ZONES**
- **DZ-FZ WAYPOINTS PARA ZONAS DE CONTROL DE VELOCIDAD / ZONAS DE SEGURIDAD**

[FR] DZ (Début de Zone de contrôle de vitesse/Zone de sécurité)

Un waypoint est utilisé pour assurer la sécurité des concurrents et/ou du public. Il définit le début d'une Zone de contrôle de vitesse ou de sécurité.

FZ (Fin de Zone de contrôle de vitesse/Zone de sécurité)

Ce waypoint est utilisé pour définir l'arrivée de la Zone de Contrôle de vitesse ou de Sécurité.

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[EN] DZ (Start Speed Control/Safety Zone) A waypoint is used to ensure competitor and/or public safety. It defines the beginning of a Speed Control or Safety Zone.

FZ (Finish Speed Control/Safety Zone)

This waypoint is used to define the finish of the Speed Control or Safety Zone

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[ES] DZ (Inicio de control de velocidad/Zona de seguridad)

Un waypoint se utiliza para garantizar la seguridad de los competidores y/o del público. Define el comienzo de un Control de Velocidad o Zona de Seguridad.

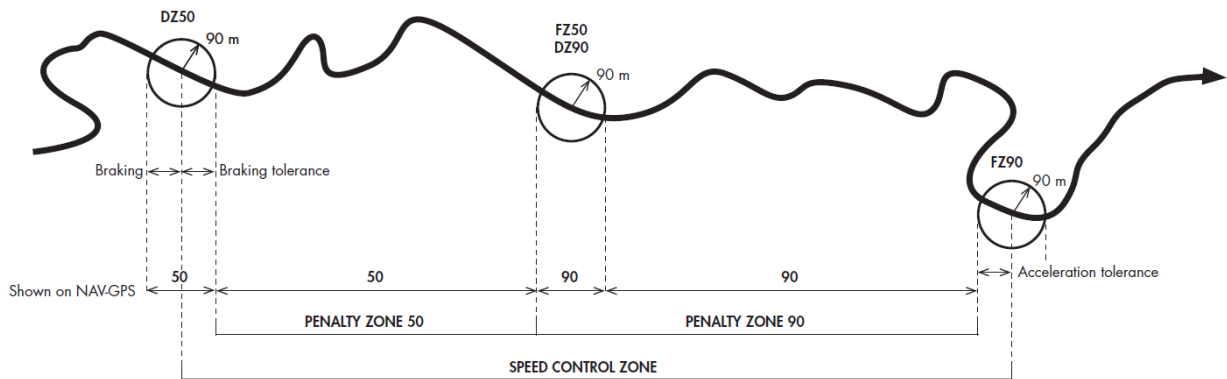
FZ (Final de control de velocidad/Zona de seguridad)

Este waypoint se utiliza para definir el final del Control de Velocidad o Zona de Seguridad.

[FR] Dans le cas d’une accélération, la limite de vitesse la plus élevée sera appliquée 90 mètres avant le waypoint FZ/DZ intermédiaire, voir diagramme ci-après:

[EN] In the case of an acceleration, the higher speed limit Will be enforced from 90 metres before the intermediate FZ/DZ waypoint, see diagram below:

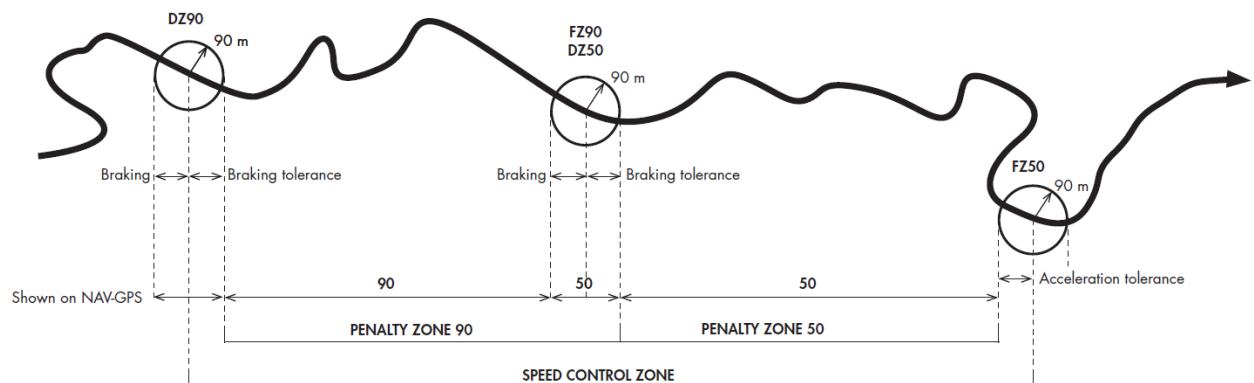
[ES] En caso de aceleración, el límite de velocidad superior se aplicará a partir de 90 metros antes del waypoint intermedio FZ/DZ, véase el siguiente diagrama:



[FR] Dans le cas d’une décélération, la limite de vitesse la moins élevée ne sera appliquée que 90 mètres après le waypoint FZ/DZ intermédiaire, voir diagramme ci-après :

[EN] In the case of deceleration, the lower speed limit will not be enforced until 90 meters after the intermediate FZ/DZ waypoint, see diagram below:

[ES] En caso de desaceleración, el límite de velocidad inferior no se aplicará hasta 90 metros después del waypoint intermedio FZ/DZ, véase el siguiente diagrama:



- **WAYPOINTS POUR LES ZONES DE NEUTRALISATION / WAYPOINTS FOR NEUTRALISATION ZONES / WAYPOINTS PARA ZONAS DE NEUTRALIZACIÓN**

[FR] NEU IN (Début de Zone de neutralisation)

Ce waypoint est utilisé pour définir le début d'une Zone de neutralisation

NEU OUT (Fin de Zone de neutralisation) :

Ce waypoint est utilisé pour définir la fin d'une Zone de neutralisation

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[EN] NEU IN (Start Neutralisation zone):

This waypoint is used to define the beginning of a neutralisation zone

NEU OUT (Finish Neutralisation zone):

This waypoint is used to define the end of the neutralisation zone

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[ES] NEU IN

Este waypoint se utiliza para definir el inicio de una zona de neutralización

NEU OUT

Este waypoint se utiliza para definir el final de la zona de neutralización